

Highway width reduced, compared to the approved scheme, to remain within the existing carriageway. This results in the retention of existing pavement widths. The staggered pedestrian crossing and island to the east of Ordnance Road is replaced with a straight across pedestrian crossing. New vehicle tables at junctions with Ordnance Road and Mary Street. Minor relocation of westbound bus stops and removal of two eastbound bus stops.

Staggered pedestrian crossing and central island replaced with straight across pedestrian crossing. Width of highway reduced overall. Eastbound road lanes revised to provide three lanes, including two with a right turn into Beckton Road.

Staggered pedestrian crossing replaced with straight across pedestrian crossing. Road lane layout reversed to provide two lanes to A13 (southeast direction) and one lane in the northwest direction. Carriageway widened at corner of Barking/Beckton Road, but narrowed on Beckton Road to remain within the existing highway. Kerb realigned on south eastern corner of junction.

Staggered pedestrian crossing and central island replaced with straight across pedestrian crossing. Width of highway reduced on the northern side of Barking Road to remain within the existing highway.

- Notes:
1. All dimensions in metres unless otherwise stated
 2. For details of site clearance refer to drawing series B1218300A13BR200.
 3. For details of drainage and ducting refer to drawing series B1218300A13BR500.
 4. For details of pavement types refer to drawing series B1218300A13BR700.
 5. For details of kerbing types refer to drawing series B1218300A13BR1100.
 6. For details of traffic signs and road markings refer to drawing series B1218300A13BR1200.
 7. For details of traffic signals refer to drawing series B1218300A13BR1200.
 8. For details of lighting columns, illuminated signs and ducts refer to drawing series B1218300A13BR1300.
 9. For details of finishes and landscaped areas refer to landscape architects drawings 361.303, 361.305, 361.307, 361.327 and 361.601.

- Existing lighting column to be retained
- ⊕ New lighting column
- ⊖ Redundant lighting column
- Advanced cycle stop lines
- Current Cycle lanes
- ⊕ Lighting Column with new electroluminescent traffic sign
- Chalmage

These plans have been provided to illustrate the principal amendments to the approved scheme (planning permission ref. LTGDC-08-171-FUL) and are provided for information purposes only.

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P1	28/10/2009	For Planning Application	REA	GL	MM
P0	13.10.08	First Issue	PDF		
Rev	Rev. Date	Purpose of revision	Drawn	Checked	Approved



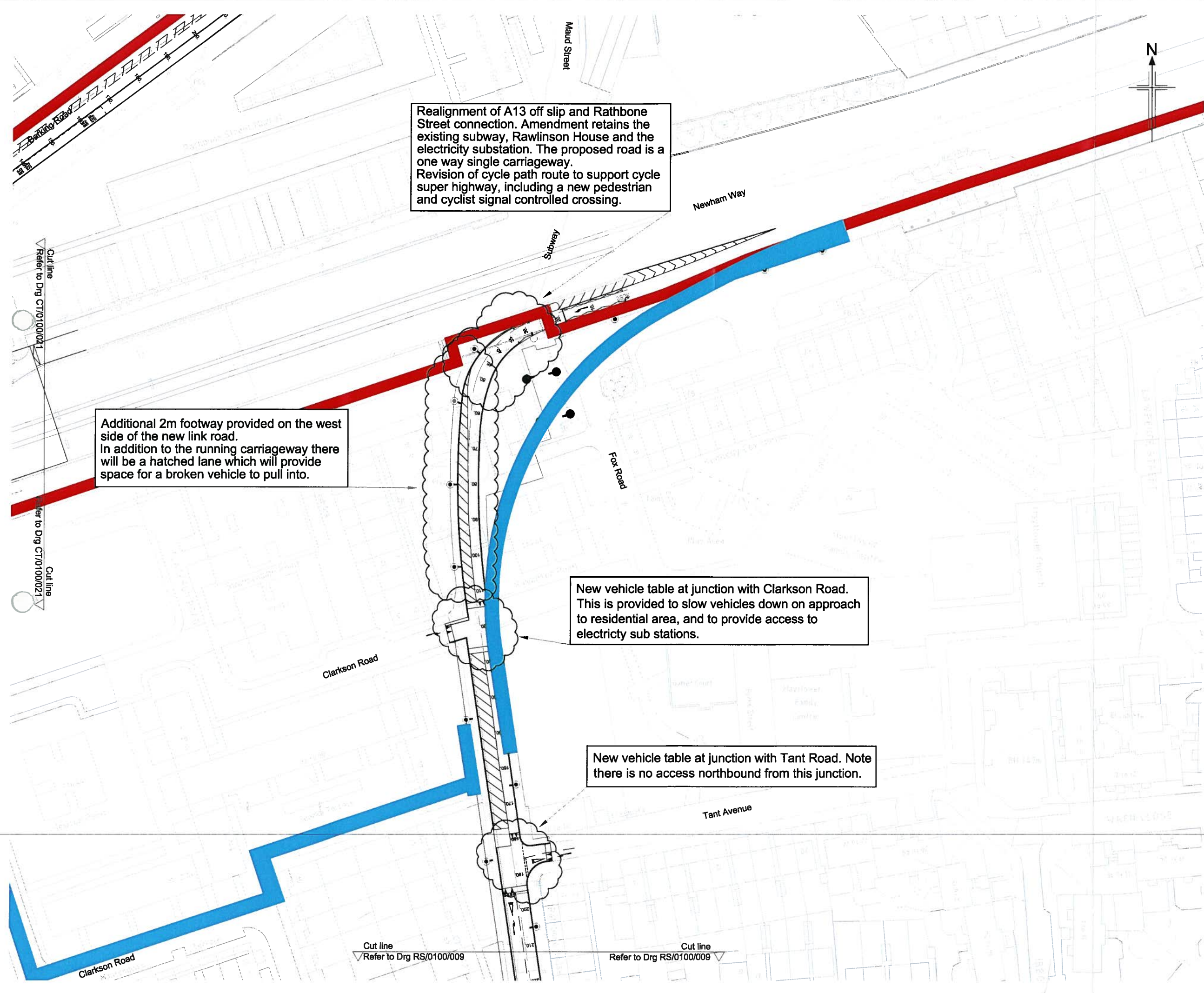
Client: **Newham**
Project: **A13 Canning Town Improvement Scheme**

Drawing Title: **Summary of principal amendments to the previously approved scheme Workstream 1**

Drawing Status:	FOR INFORMATION	
Scale:	1:500 @ A1	
Jacobs No.:	B1218300	
Drawing Number:	B1218300/A13/BR/100/010	Rev P1

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Realignment of A13 off slip and Rathbone Street connection. Amendment retains the existing subway, Rawlinson House and the electricity substation. The proposed road is a one way single carriageway. Revision of cycle path route to support cycle super highway, including a new pedestrian and cyclist signal controlled crossing.

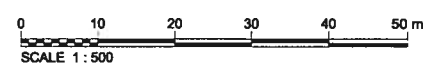
Additional 2m footway provided on the west side of the new link road. In addition to the running carriageway there will be a hatched lane which will provide space for a broken vehicle to pull into.

New vehicle table at junction with Clarkson Road. This is provided to slow vehicles down on approach to residential area, and to provide access to electricity sub stations.

New vehicle table at junction with Tant Road. Note there is no access northbound from this junction.

- Notes:
- All dimensions in metres unless otherwise stated
 - To be viewed in conjunction with landscape architects drawings drawings 361.309, 361.311, 361.313
- key:
- New Lighting Column
 - Existing Lighting Column to be Retained
 - Advanced stop lines
 - Current Cycle Lane
 - Original Cycle Lane

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P1	28/10/2008	For Information	IMC	GL	MM
PD	13/10/2008	First Issue	PDF		
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RINGWAY JACOBS
Integrated expertise

Client: **Newham**

Project: **A13 Canning Town Improvement Scheme**

Drawing Title: **Summary of principal amendments to the previously approved scheme Workstream 2**

Drawing Status: **FOR INFORMATION**

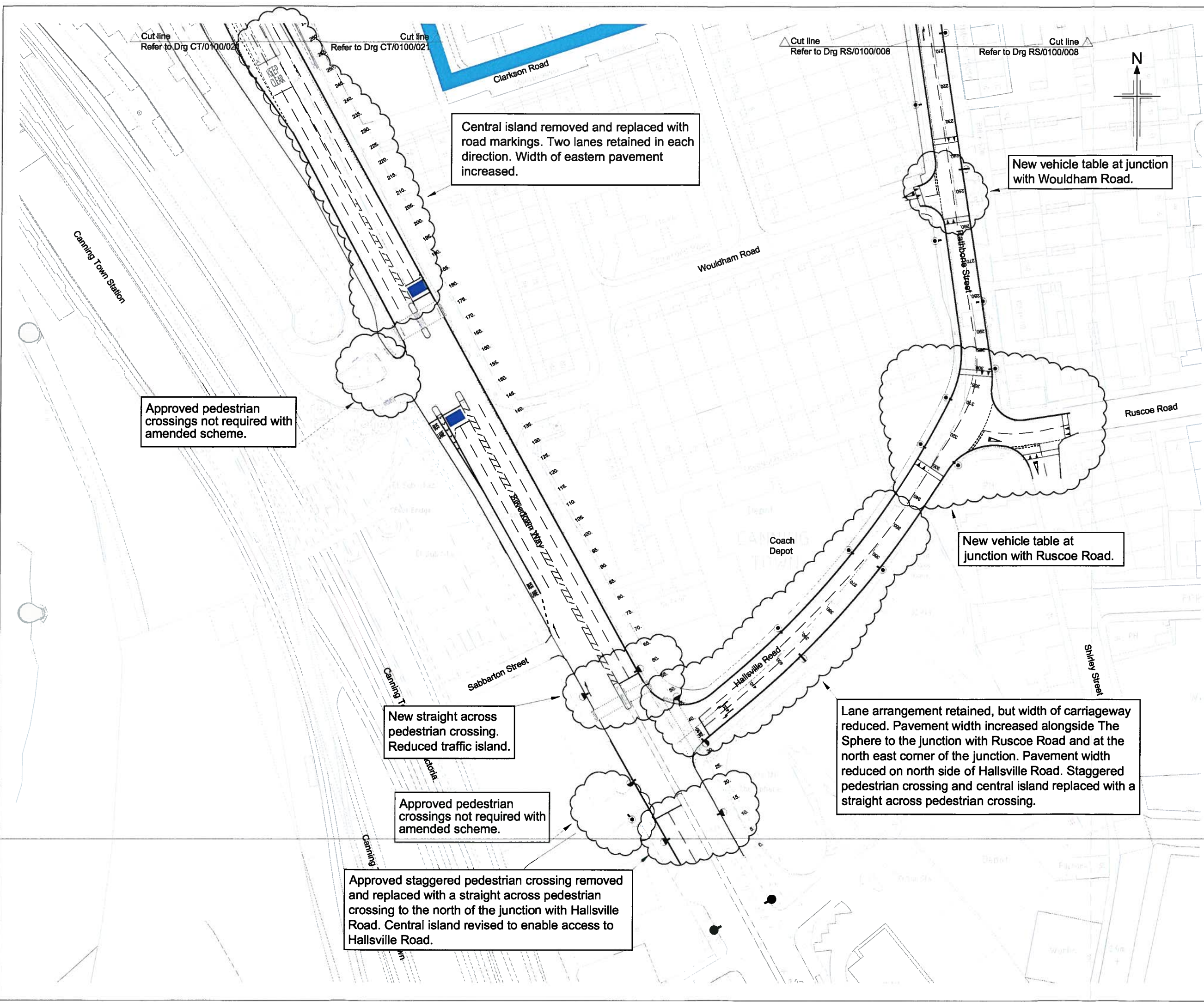
Scale: 1:500 @ A1

Jacobs No.: B1218300

Client No.:

Drawing Number: **B1218300/A13/RS/0100/008** Rev: **P1**

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Cut line Refer to Drg CT/0100/021

Cut line Refer to Drg CT/0100/021

Cut line Refer to Drg RS/0100/008

Cut line Refer to Drg RS/0100/008



Central island removed and replaced with road markings. Two lanes retained in each direction. Width of eastern pavement increased.

New vehicle table at junction with Wouldham Road.

Approved pedestrian crossings not required with amended scheme.

New vehicle table at junction with Ruscoe Road.

New straight across pedestrian crossing. Reduced traffic island.

Lane arrangement retained, but width of carriageway reduced. Pavement width increased alongside The Sphere to the junction with Ruscoe Road and at the north east corner of the junction. Pavement width reduced on north side of Hallsville Road. Staggered pedestrian crossing and central island replaced with a straight across pedestrian crossing.

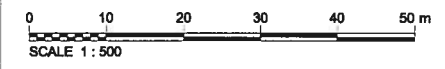
Approved pedestrian crossings not required with amended scheme.

Approved staggered pedestrian crossing removed and replaced with a straight across pedestrian crossing to the north of the junction with Hallsville Road. Central island revised to enable access to Hallsville Road.

- Notes:
- All dimensions in metres unless otherwise stated
 - To be viewed in conjunction with landscape architects drawings 361.309, 361.311, 361.313

- key:
- New Lighting Column
 - Existing Lighting Column to be Retained
 - New 10m galvanised steel tubular lighting column
 - Existing column to be replaced by 8m galvanised steel tubular lighting column
 - Advanced stop lines
 - Current Cycle Lane
 - Original Cycle Lane

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Rev	Rev. Date	Purpose of revision	Drawn	Checked/Approved	

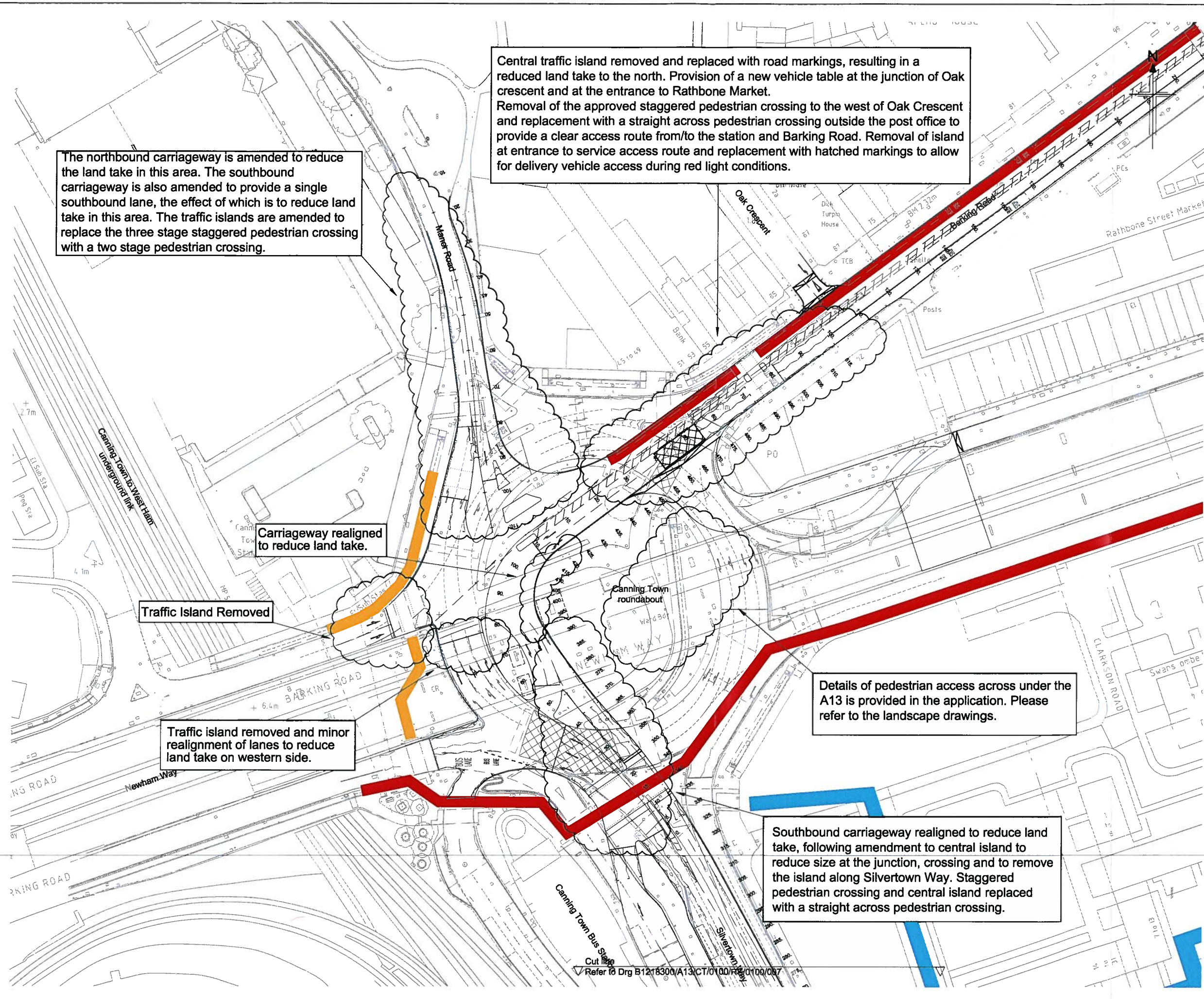


Client: Newham
Project: A13 Canning Town Improvement Scheme

Drawing Title: Summary of principal amendments to the previously approved scheme Workstream 2

Drawing Status	FOR INFORMATION	
Scale	1:500 @ A1	
Jacobs No.	B1218300	
Client No.		
Drawing Number	B1218300/A13/RS/0100/009	Rev P1

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Central traffic island removed and replaced with road markings, resulting in a reduced land take to the north. Provision of a new vehicle table at the junction of Oak Crescent and at the entrance to Rathbone Market. Removal of the approved staggered pedestrian crossing to the west of Oak Crescent and replacement with a straight across pedestrian crossing outside the post office to provide a clear access route from/to the station and Barking Road. Removal of island at entrance to service access route and replacement with hatched markings to allow for delivery vehicle access during red light conditions.

The northbound carriageway is amended to reduce the land take in this area. The southbound carriageway is also amended to provide a single southbound lane, the effect of which is to reduce land take in this area. The traffic islands are amended to replace the three stage staggered pedestrian crossing with a two stage pedestrian crossing.

Carriageway realigned to reduce land take.

Traffic Island Removed

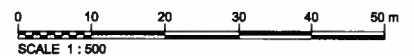
Traffic island removed and minor realignment of lanes to reduce land take on western side.

Details of pedestrian access across under the A13 is provided in the application. Please refer to the landscape drawings.

Southbound carriageway realigned to reduce land take, following amendment to central island to reduce size at the junction, crossing and to remove the island along Silvertown Way. Staggered pedestrian crossing and central island replaced with a straight across pedestrian crossing.

- KEY**
- Advanced stop lines
 - Cycle Lane (on carriageway)
 - Cycle Lane (on footway)
 - Original cyclepath

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PI	28/10/2009	For Information	REA	GL	MM
PD	13/10/2009	For Information	ILC		
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A13 Canning Town Improvement Scheme

Summary of principal amendments to the previously approved scheme Workstream 3

Drawing Status: **FOR INFORMATION**

Scale: 1:500 @ A1
 Jacobs No.: B1218300
 Client No.:

Drawing Number: **B1218300/A13/CT/0100/021A** Rev: **P1**

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Refer to Drg B1218300/A13/CT/0100/R02/0100/007